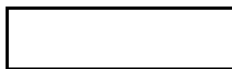




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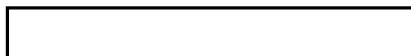
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PHOTOGRAPHIC INTELLIGENCE REPORT

VITAL RECORDS COPY

STATUS OF NORTH VIETNAMESE RAILROADS



DECLASS REVIEW by NIMA/DOD

REPORT NO. 25
CIA/PIR 75082

DATE JANUARY 1967

GROUP 1
Excluded from automatic
downgrading and declassification

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WARNING

This document contains information affecting the national defense of the United States, within the meaning of Title 18, sections 793 and 794, of the U.S. Code, as amended. Its transmission or revelation of its contents to or receipt by an unauthorized person is prohibited by law.

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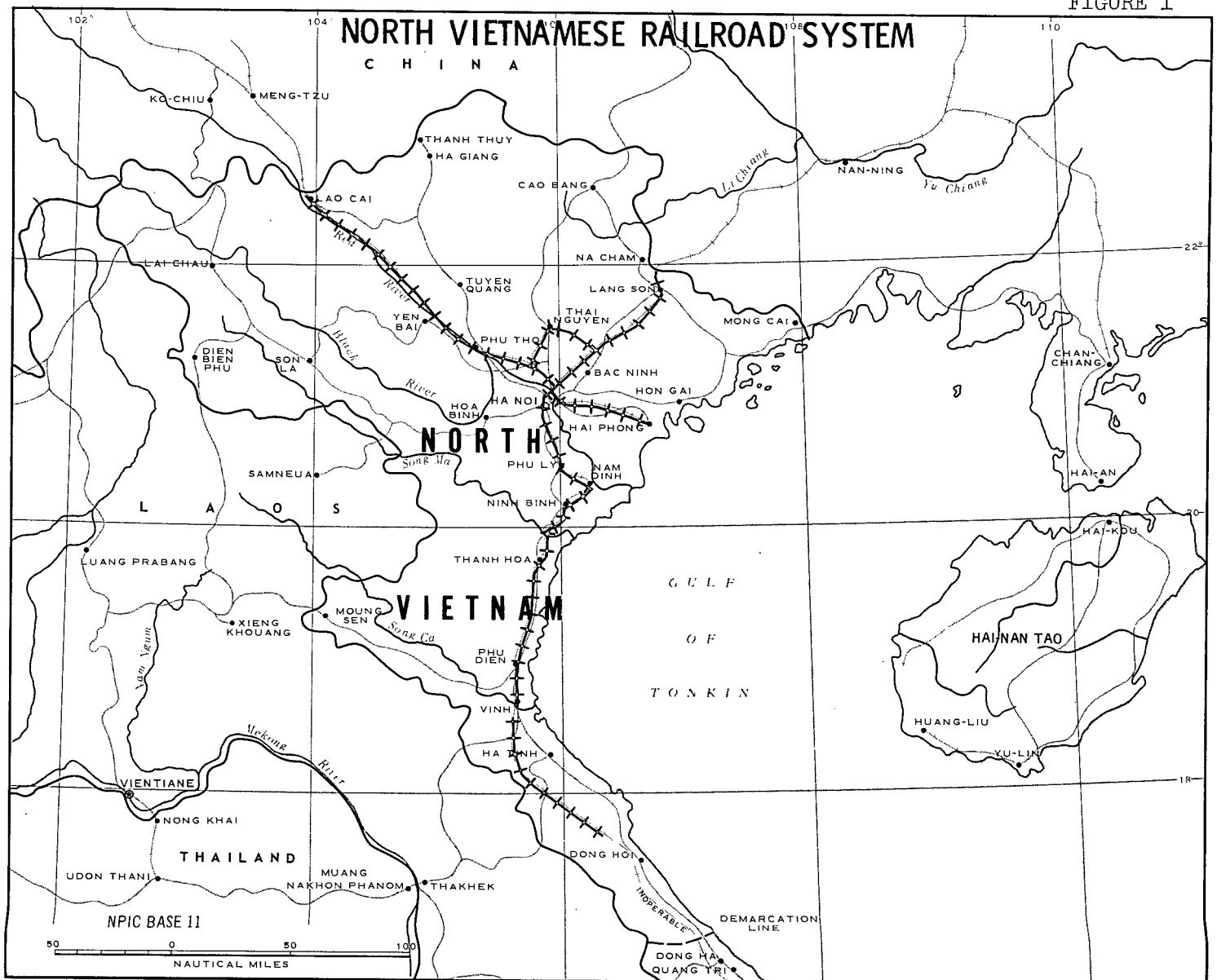
P R E F A C E

This report summarizes significant items of photo intelligence relating to the North Vietnamese railroad system which has been developed from available photography (received by the CIA Imagery Analysis Division) during the period indicated on the cover. This report is intended to provide a comprehensive view of the railroad system and does not necessarily reflect the most recent status changes as reported by pilots' reports or field photo intelligence cables.

The railroad system has been broken down into 5 major lines: the Hanoi/Lao Cai Line, the Hanoi/Dong Dang Line, the Hanoi/Haiphong Line, the Hanoi/Dong Hoi Line and the Thai Nguyen Line (Figure I). Annotated maps and photos are used to show the status of each line.

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SUMMARY

That portion of the Hanoi/Haiphong Rail Line covered by interpretable photography was open to through traffic. Only 1 bridge interdiction was observed on the Hanoi - Vinh segment of the Hanoi/Dong Hoi Rail Line, and considerable by-pass construction activity has been noted on the southern segment (i. e., from Dong Bai to Xom Khe).

CONTENTS

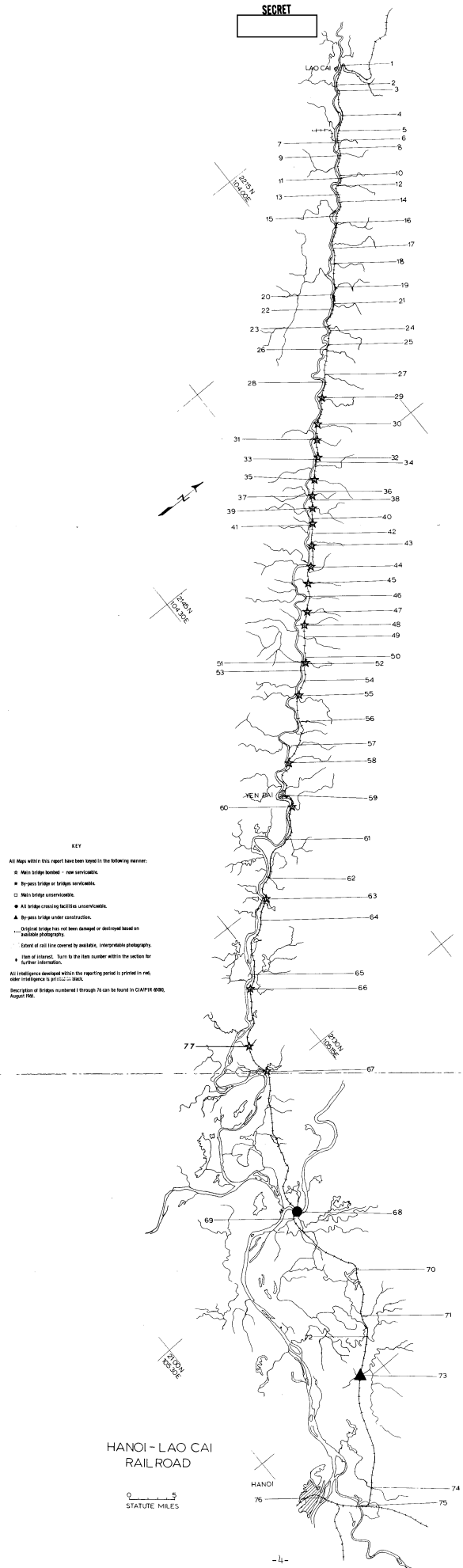
Hanoi/Lao Cai Rail Line	Page	4
Hanoi/Dong Dang Rail Line		5
Hanoi/Dong Hoi Rail Line		7
Hanoi/Haiphong Rail Line		18
Thai Nguyen Rail Line		19

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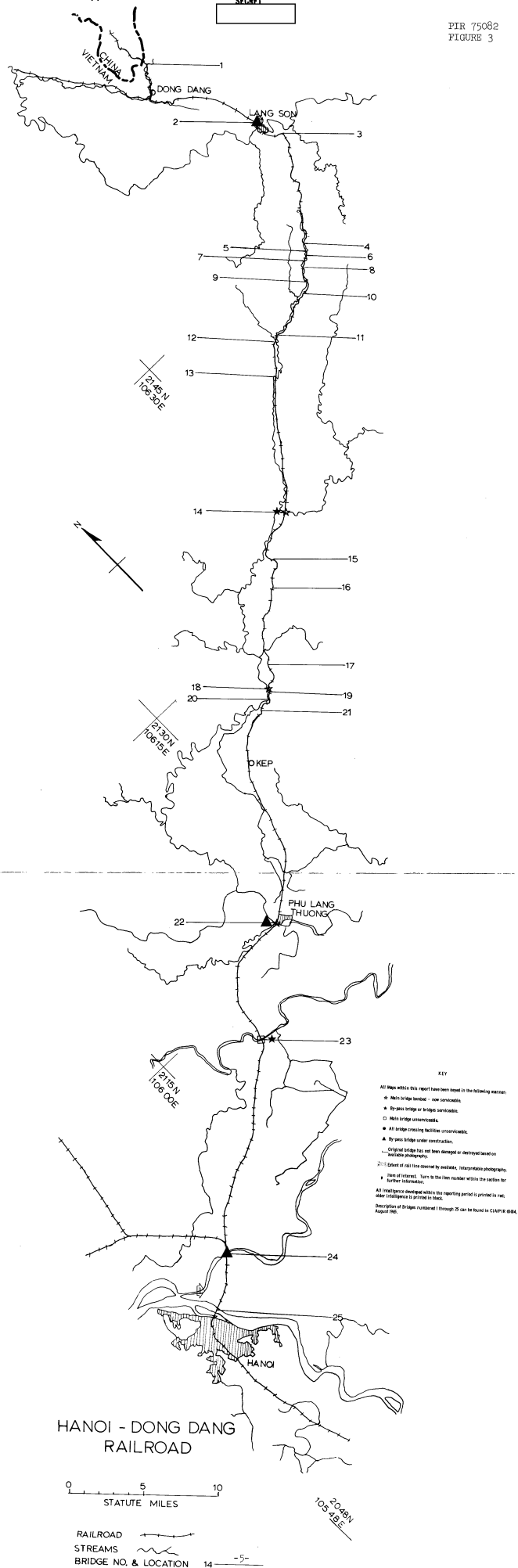
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FIGURE 2



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FIGURE 3



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ITEM OF INTEREST NO. 1: Hanoi/Dong Dang Rail Line

NAME : Railroad Ferryboat

BE : NA

COORDINATES : 21 03N - 105 52E, WJ907294

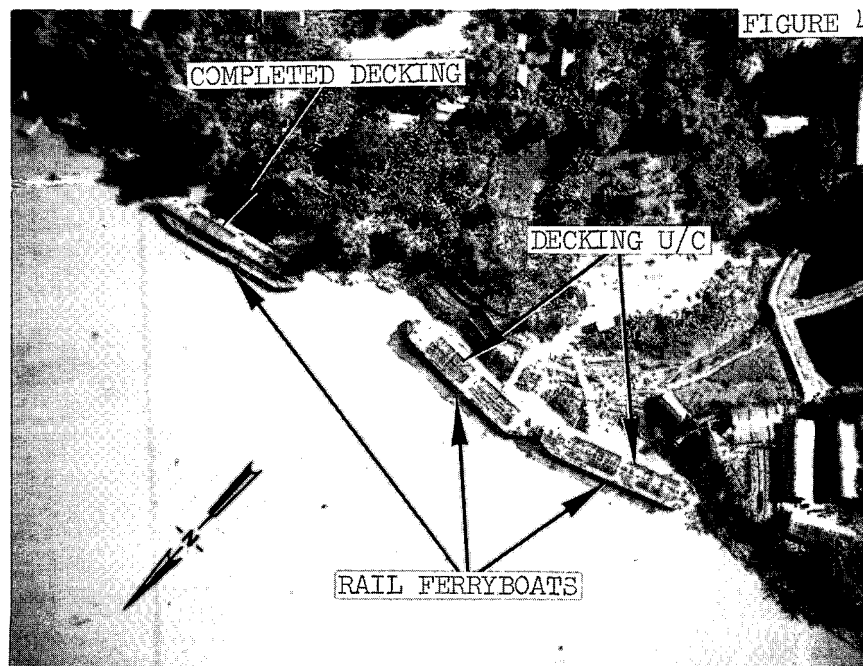
PHOTO REFERENCE :

MAP REFERENCE : JOG (A) 1501, NF 48-11

DOCUMENTS : NPIC Briefing Board L-2505, 4 January 1967

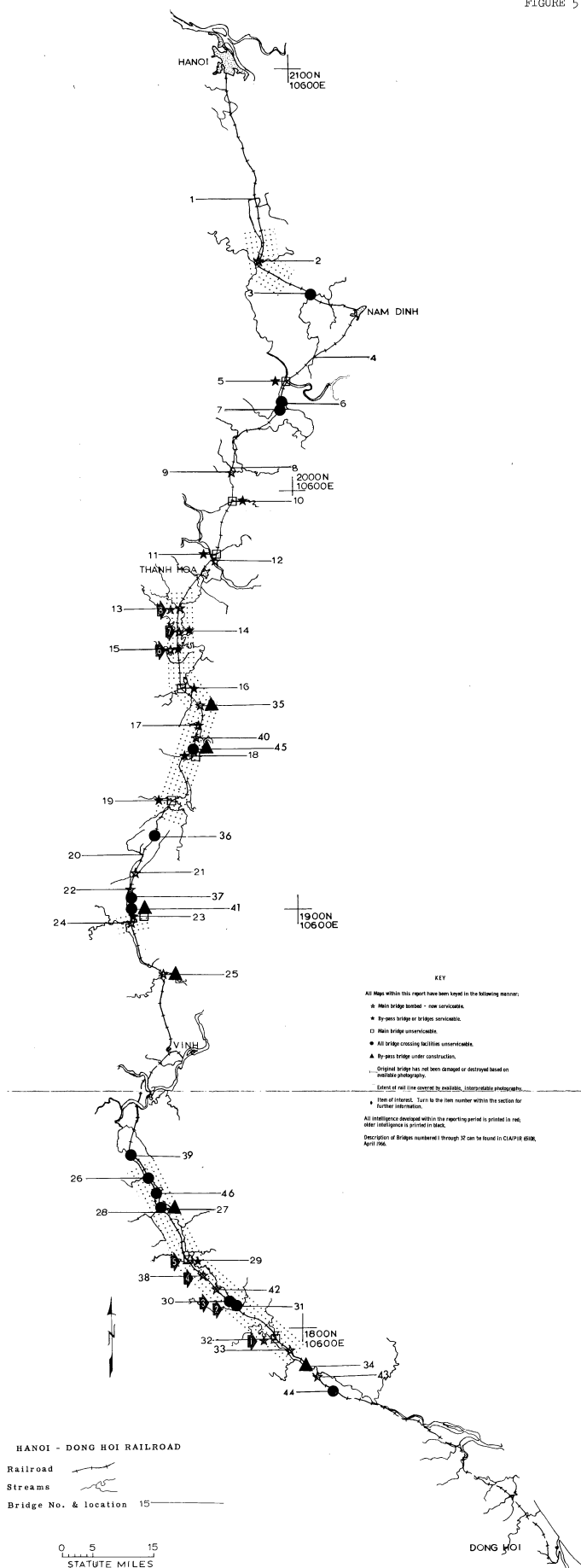
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Three barges are observed at a ferry fitting out facility approximately 1.7 nm NW of Hanoi on the southern bank of the Canal des Rapides. The barges are being fitted out as rail ferryboats and are currently in the mid to late stages of modification. Braces have been fitted and deck construction has commenced on 2 of the barges. The third barge will be serviceable when rail has been laid on its newly constructed deck. These craft are located approximately 5.8 nm by water from the by-pass ferry site under construction around Hanoi (see Report No. 19) and consequently may be intended for use at that site.

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FIGURE 5



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ITEM OF INTEREST NO. 1: Hanoi/Dong Hoi Rail Line

NAME : Xom Khe RR. By-pass Bridge

BE : NA

COORDINATES : 17 58N - 105 56E, WF987980

PHOTO REFERENCE :

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MAP REFERENCE : JOG (A) 1501, NE 48-11

The Xom Khe Railroad By-pass Bridge has been completed and is serviceable. A second bridge has been constructed on the by-pass approach over a broad depression and is also serviceable. Both the by-pass bridge and the approach bridge have deck-beam spans supported by A-frames.

The "shuttle" bridge at the main bridge site remains serviceable. It is constructed in the same manner as the by-pass bridges, but does not connect with the rail line (see Report No. 2). The original main bridge was destroyed.

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Several stacks of spare rails (one stack included in photography) were noted in the general area of the bridge.

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FIGURE 6

APPROACH BRIDGE

BY-PASS BRIDGE

RAILS

SERVICEABLE SHUTTLE

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[REDACTED]

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ITEM OF INTEREST NO. 2: Hanoi/Dong Hoi Rail Line

NAME : Bai Duc Thon RR. Bridge S

BE 25X1 : [REDACTED]

COORDINATES : 18 03N - 105 50E, WE886964

PHOTO REFERENCE : [REDACTED]

MAP REFERENCE : JOG (A) 1501, NE 48-7

25X1

The Bai Duc Thon Railroad Bridge S is undergoing active reconstruction and is probably still unserviceable. A-frames have been erected over the destroyed portion of the bridge, and a possible prefabricated deck is located on the northwest approach. The main bridge [REDACTED] remains unserviceable.

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[REDACTED]

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FIGUR

POSSIBLE PREFABRICATED DECK

UNSERVICEABLE MAIN BRIDGE

ERECTED A-FRAMES

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CIA IMAGERY ANALYSIS DIVISION

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ITEM OF INTEREST NO. 3: Hanoi/Dong Hoi Rail Line

NAME : La Khe Thon RR. By-pass Bridge

BE : NA

COORDINATES : 18 03N - 105 49E, WE872973

PHOTO REFERENCE : [REDACTED] 25X1

MAP REFERENCE : JOG (A) 1501, NE 48-7

DOCUMENTS : National Intelligence Survey, Section 31, North Vietnam

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The La Khe Thon Railroad By-pass Bridge is undergoing active reconstruction; the destroyed section of the bridge is being repaired. New piers have been erected, but replacement bridge spans are not observed in the area. Probable rails approximately [REDACTED] in length are located in the water on and adjacent to the highway ford (the length of rails in North Vietnam varies from [REDACTED] [REDACTED] The presence of surplus rails at this location, at Xom Khe (see page 8) and at several locations in between indicates that there is no scarcity of rails along this railroad segment at this time. 25X1

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FIGURE 8

UNSERVICEABLE MAIN BRIDGE

RAILS

PIERS EMPLACED

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ITEM OF INTEREST NO. 4: Hanoi/Dong Hoi Rail Line

NAME : Roadbed Repair

BE : NA

COORDINATES : 18 08N - 105 44E, WF782057

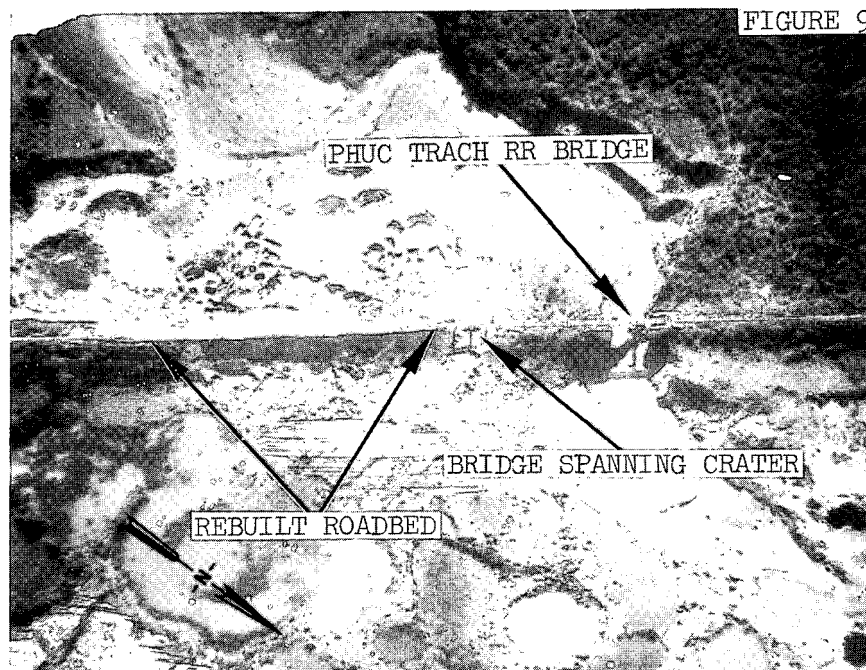
PHOTO REFERENCE

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MAP REFERENCE : JOG (A) 1501, NE 48-7

Probable heavy cratering of the roadbed adjacent to the serviceable 2-span, deck-beam Phuc Trach Railroad Bridge [] is being repaired. One large crater has been spanned by a 4-span deck-beam bridge, and the adjacent roadbed is being completely rebuilt. There is no rail on the new section of roadbed at this time.

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ITEM OF INTEREST NO. 5: Hanoi/Dong Hoi Rail Line

NAME : Dong Bai RR. Bridge

BE 25X1 :

COORDINATES : 18 09N - 105 43E, WF762081

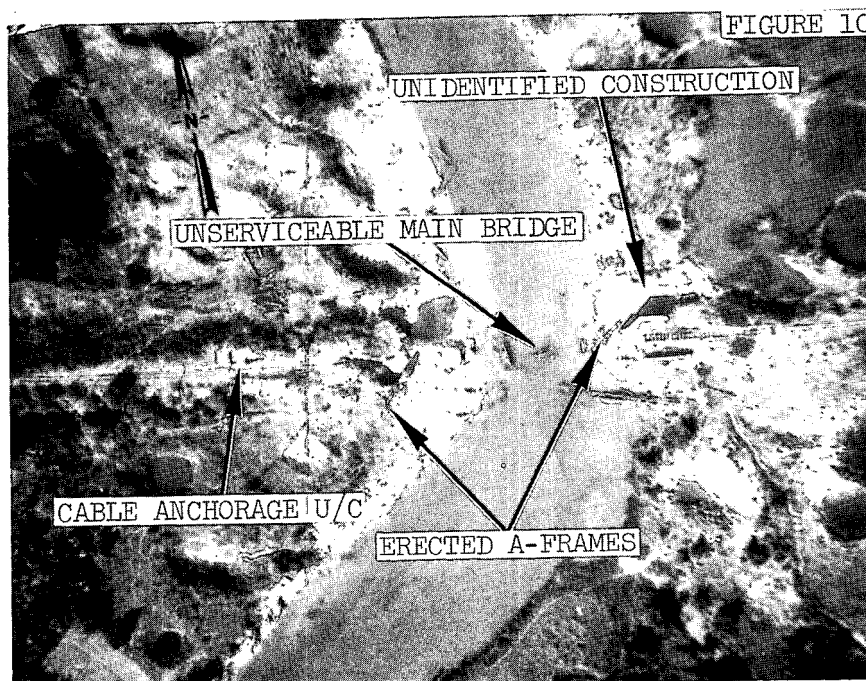
PHOTO REFERENCE :

MAP REFERENCE : JOG (A) 1501, NE 48-7

Construction activity has been noted at the site of the destroyed Dong Bai Railroad Bridge (Figure 10). Two unaligned A-frames have been erected, 1 on each side of the river, a cable anchorage is under construction on the western approach, and an area of unidentified construction activity is located adjacent to the eastern abutment.

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The Dong Bai Railroad By-pass Bridge N has been repaired and is serviceable (Figure 11). This bridge is of the removeable deck-beam type, and has been reconstructed in the same manner as when it was last serviceable (see Report No. 8 for a detailed description and diagram).

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FIGURE 11

REMOVEABLE DECKING

POSSIBLE WINCH

ERECTED A-FRAMES

POSSIBLE WINCH

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ITEM OF INTEREST NO. 6: Hanoi/Dong Hoi Rail Line

NAME : Trong Quang Tien RR. Bridge

BE 25X1

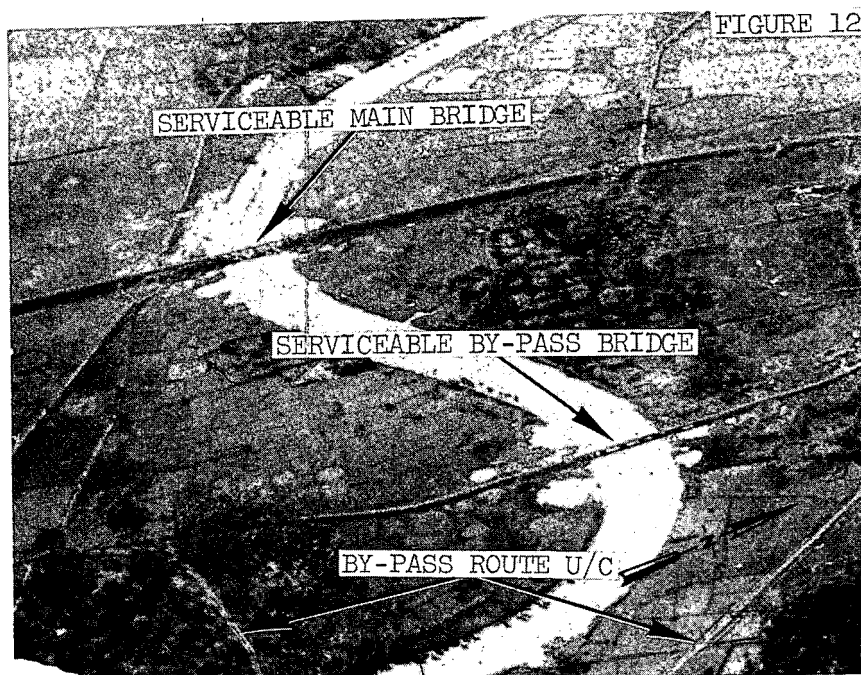
COORDINATES : 19 37N - 105 42E, WG744699

PHOTO REFERENCE

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MAP REFERENCE : JOG (A) 1501, NE 48-3

The 5-span, deck-beam Trong Quang Tien RR. Bridge has been repaired and is serviceable. The existing by-pass bridge [REDACTED] is also serviceable, and a second by-pass bridge is in the initial stages of construction.

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[REDACTED]

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ITEM OF INTEREST NO. 7: Hanoi/Dong Hoi Rail Line

NAME : Thinh Lac RR. Bridge

BE 25X1

: [REDACTED]

COORDINATES : 19 40N - 105 42E, WG741748

PHOTO REFERENCE

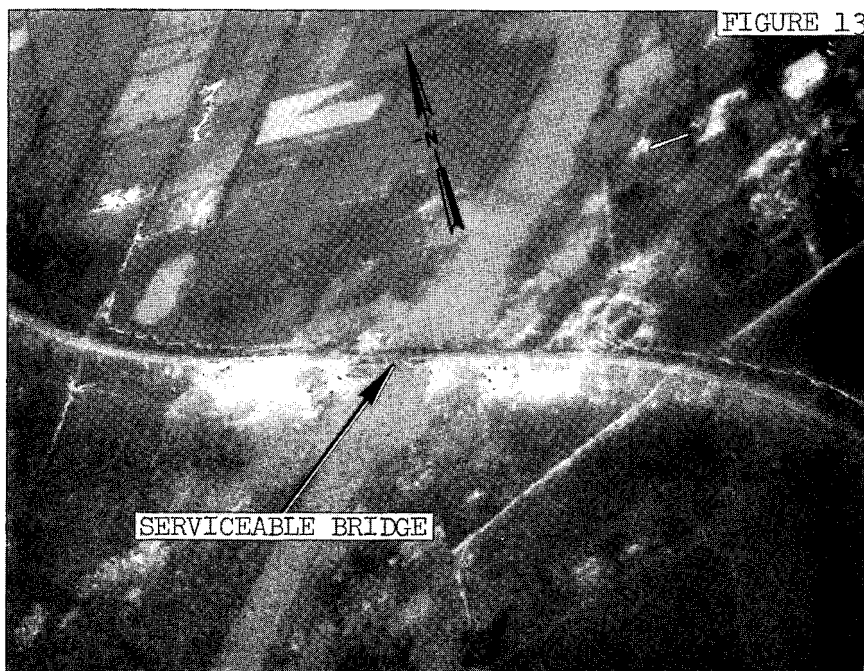
: [REDACTED]

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MAP REFERENCE : JOG (A) 1501, NE 48-3

The destroyed Thinh Lac RR. Bridge has been replaced by a 4-span steel and timber deck-beam bridge. The replacement bridge is now serviceable. The by-pass bridge, not shown, [REDACTED] remains serviceable.

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[REDACTED]

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ITEM OF INTEREST NO. 8: Hanoi/Dong Hoi Rail Line

NAME : Thien Linh Dong RR. Bridge SW

BE 25X1

COORDINATES : 19 43N - 105 42E, WG746815

PHOTO REFERENCE

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MAP REFERENCE

: JOG (A) 1501, NE 48-3

The 4-span, deck-beam Thien Linh Dong RR. Bridge SW has been repaired and is serviceable. This bridge is a by-pass for the main bridge [REDACTED] which remains serviceable. A probable second by-pass route is under construction to the west of the existing by-pass, but no evidence of bridge construction has been noted.

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FIGURE 14

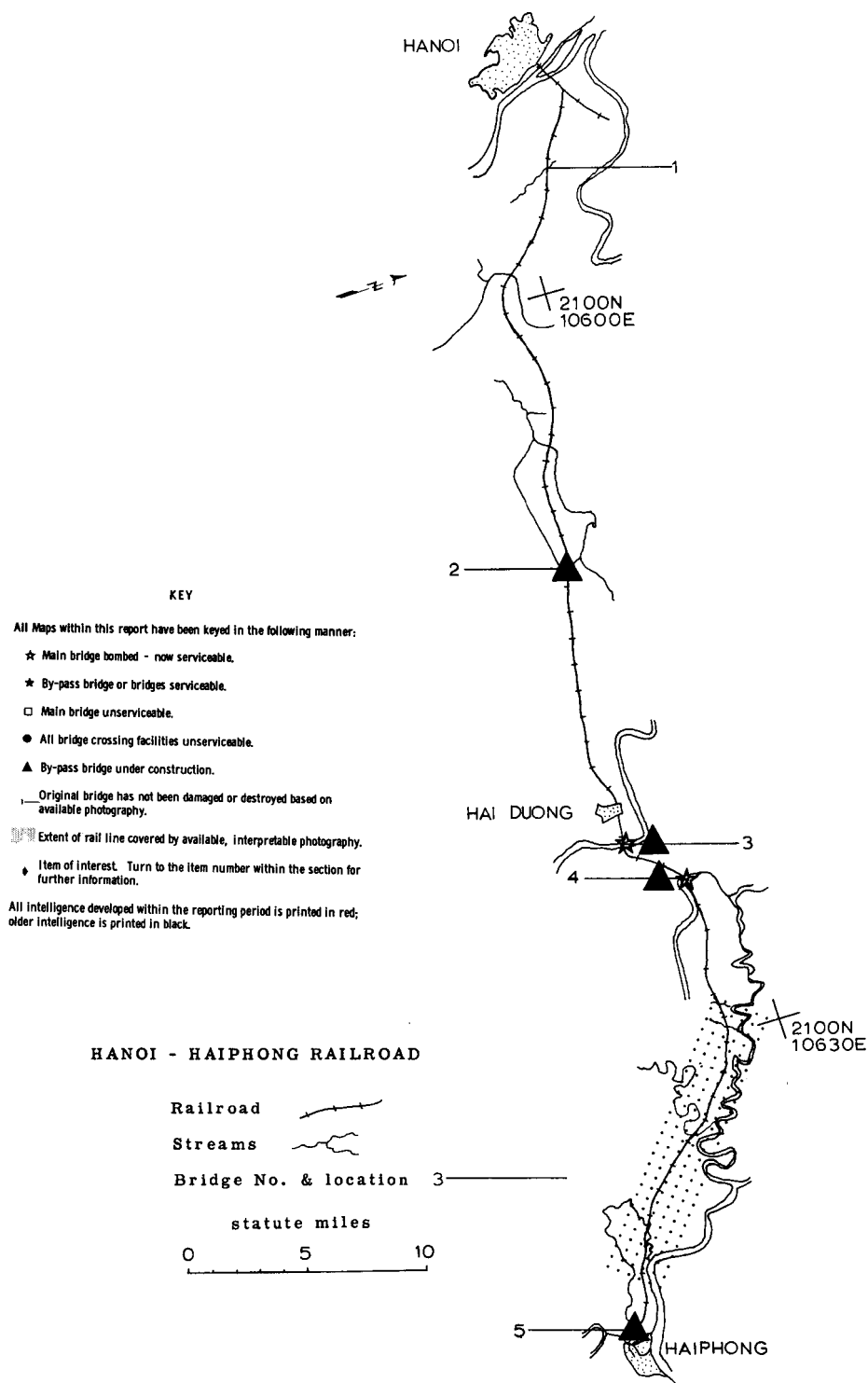
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FIGURE 15



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FIGURE 16

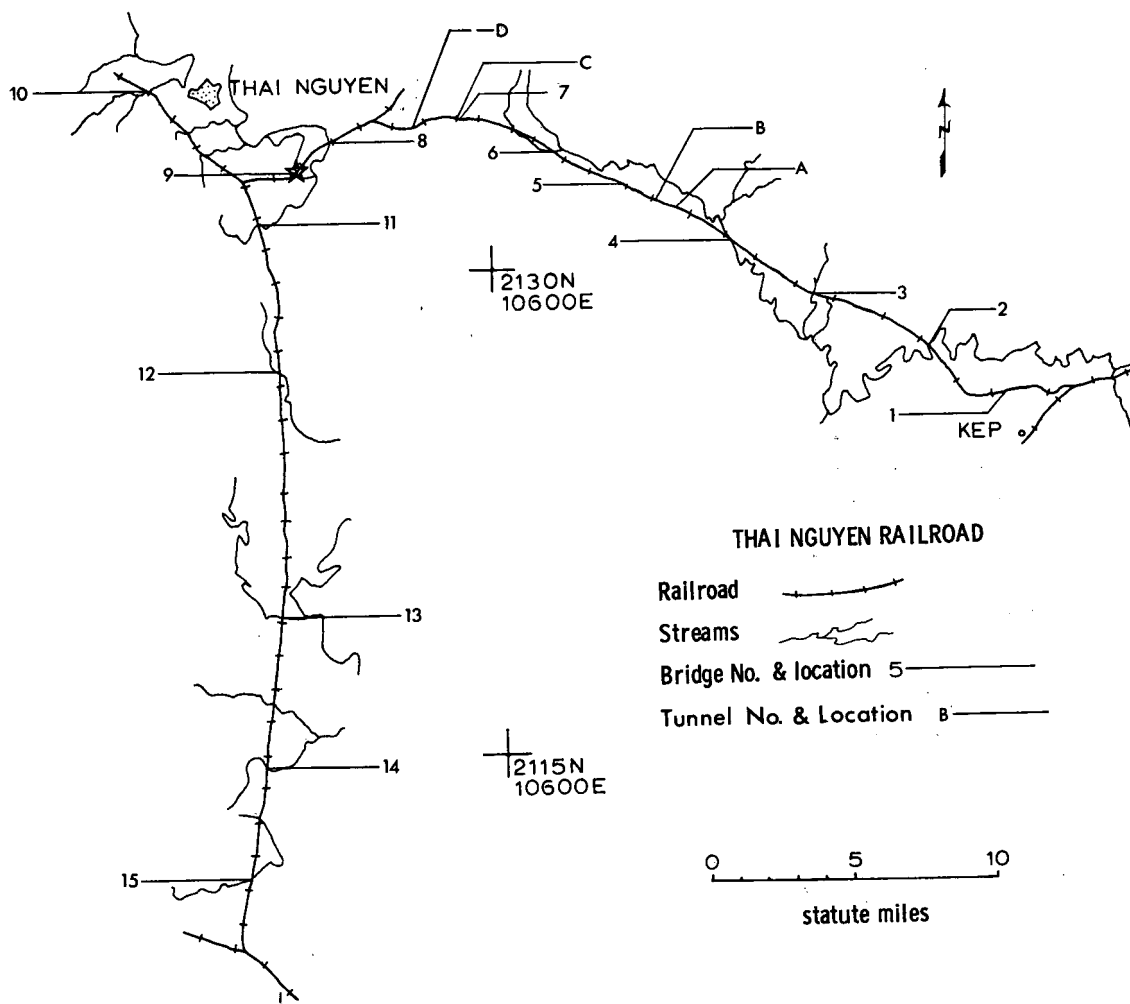
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KEY

All Maps within this report have been keyed in the following manner:

- ★ Main bridge bombed - now serviceable.
- ★ By-pass bridge or bridges serviceable.
- Main bridge unserviceable.
- All bridge crossing facilities unserviceable.
- ▲ By-pass bridge under construction.
- Original bridge has not been damaged or destroyed based on available photography.
- Extent of rail line covered by available, interpretable photography.
- Item of Interest. Turn to the item number within the section for further information.

All intelligence developed within the reporting period is printed in red; older intelligence is printed in black.



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